

ADDENDUM NO. 1

Contract: 2026-04 Sutherland Creek Bridge Rehabilitation

The Township of South Glengarry in cooperation with the United Counties of Stormont Dundas and Glengarry have added the rehabilitation of the Wood Creek Bridge to this contract.

The Wood Creek Bridge (Asset ID 02-849) is located on County Road 2 (Old Highway 2), 1.3 km east of County Road 23 and carries two lanes of traffic over the Wood Creek. The structure consists of a 9.144 m span reinforced concrete rigid frame founded on timber piles. The scope of rehabilitation work at the Wood Creek Bridge includes but is not limited to concrete removals at the joint, installation of dowels and reinforcing steel, abrasive blast cleaning of existing reinforcing, placement of polyurethane with aggregate patch material and new compression seals.

The work on the Wood Creek Bridge shall be completed prior to August 21, 2026.

The location of the bridge is shown in Figure 1 – Key Plan.

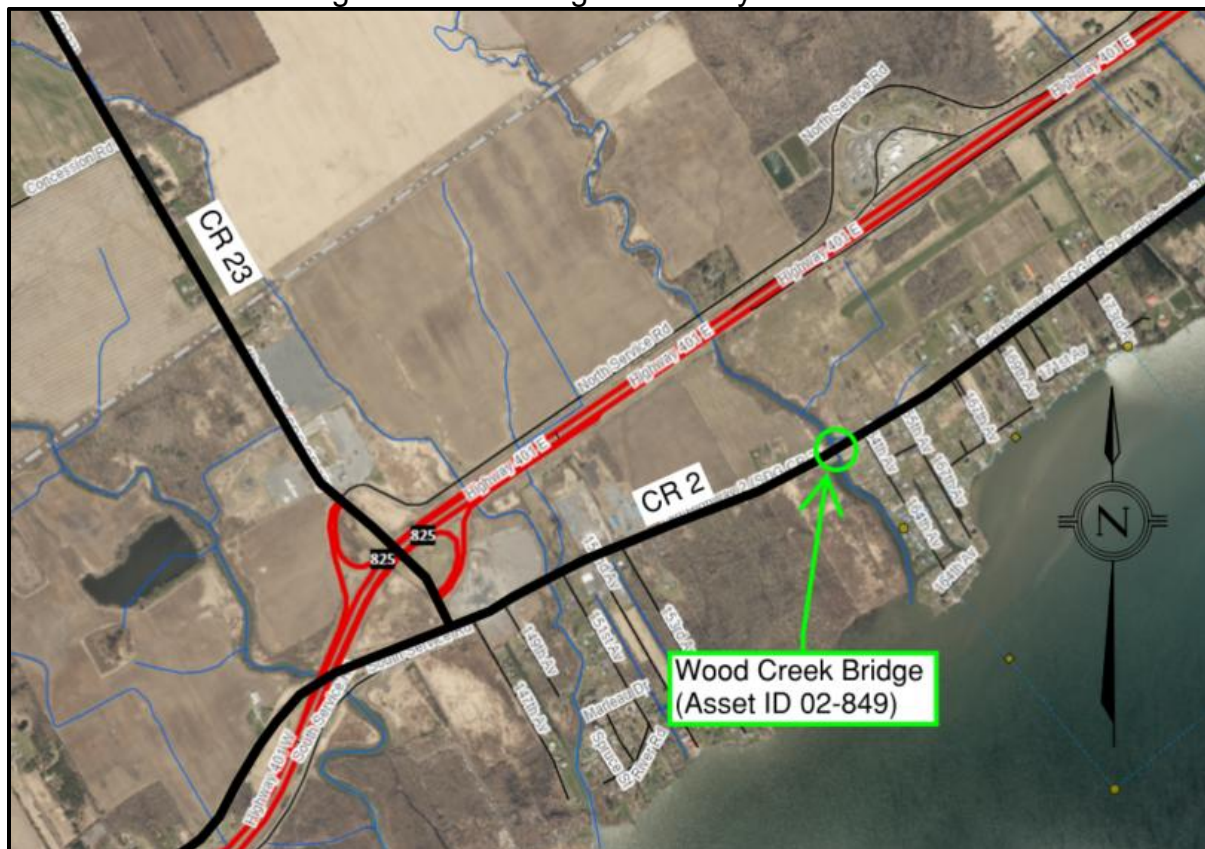


Figure 1 – Key Plan



Keystone Bridge Management Inc.
Your Bridge Asset Management Specialist

ADDENDUM COVER SHEET

**PROJECT
NAME:** 2026-04 Sutherland Creek Bridge Rehabilitation

DATE: March 25, 2026

We hereby certify that addendum 1 listed below meets the intent of the design for Contract 2026-004 for the Sutherland Creek Bridge Rehabilitation and Wood Creek Bridge Rehabilitation.

- **Addendum 1**



Sean Miller, P.Eng.
Bridge Engineer
Keystone Bridge Management Inc.



Michael Matthews, P.Eng.
Managing Director
Keystone Bridge Management Inc.

Section A: Form of Tender, "Description of Work" is amended by additional of the following:

Section A: Form of Tender

**DESCRIPTION OF
WORK:**

Rehabilitation of joints on single span rigid frame bridge (ID 02-849) located on County Road 2 over the Wood Creek approximately 1.3 km east of County Road 23. Work generally includes but is not limited to staged traffic control for concrete removals at joint, installation of dowels and under a full road closure; abrasive blast cleaning of reinforcing steel, installation of polyurethane with aggregate patching material and new compression seals.

Section E: Special Provisions are amended by addition of the following:

Section E: Special Provisions

SP14 – SITEWORK, INCLUDING MOBILIZATION, DEMOBILIZATION – WOOD CREEK BRIDGE

The following work, activities and costs shall be included in this item:

- a) Bonds, administration and supervisory costs.
- b) Mobilization and demobilization of equipment material and Contractor's forces.
- c) Schedules and cash flow forecast.
- d) Site preparation for work.
- e) Costs of all necessary approvals and permits as applicable.
- f) Cost of building construction depot (laydown area) in a location acceptable to the Owner.
- g) All costs associated with locates, protection or temporary relocation and reinstallation of utilities, if applicable and not specified in other areas of the contract.
- h) Installation of temporary barricades, hoarding, fencing and other protection required.
- i) Cost of supplying and maintenance of adequate sanitary facilities.
- j) Scaffolding and other required access to perform the work, if not specified in other areas of the contract.
- k) Costs associated with submission of shop drawings, rebar schedules, product submissions etc., including engineering and QVE.
- l) Cost of advertising of Substantial Completion in Daily Commercial News.
- m) Costs associated with access to private properties as required and obtaining written release from the affected property Owners. Copy to be submitted to the Engineer.
- n) Maintaining and reinstatement of existing road signs, mailboxes, etc.
- o) All costs of any incidental work not specifically mentioned in the Contract Documents but required by virtue of the work.
- p) All costs associated with required environmental protection measures not specified elsewhere in the Contract.
- q) Contractor's office.
- r) Fencing of the Work area.
- s) Brushing around the structure. All brush and trees within 3m of the wingwall and under the structure are to be removed down to ground level.
- t) Cost of reinstating to their original conditions the Contractor's laydown areas and office sites.
- u) All costs associated with preparation of as-built drawings (red line hard copy mark-ups).
- v) All costs associated with field surveying as specified in the Contract Documents and as required in order to properly layout and complete the work.
- w) All costs associated with biologist sweeps of the site.
- x) Maintenance of bird netting (if installed) and removal of bird netting once construction is complete.
- y) Clearing and grubbing required for site access in accordance with OPSS.MUNI 201.

The Contractor shall submit a detailed Work Schedule and, if requested, a Projected Monthly Cash Flow Forecast to the Engineer at the Pre-Construction Meeting. The Projected Monthly Cash Flow Forecast shall indicate the projected cost of the work to be completed in each calendar

month of the Contract term and shall be based on the Contract prices and be consistent with the Detailed Work Schedule.

The Contractor is advised that no additional payment will be made for any repeated mobilization and demobilization for any of the construction activities covered by this Contract; interrupted by weather or by any other construction activity included in any part of this Contract.

Measurement for Payment:

- a) Payment for this lump sum item will be prorated over the scheduled duration of the Contract per the approved schedule.

Basis for Payment:

- a) Payment at the Contract lump sum price for this item shall be full compensation for all labour, materials, equipment and transportation required to complete the work. Contractor shall bear all immediate, subsequent and consequential costs associated with change in the schedule, staging, and methodology of the work.

SP15 – TRAFFIC CONTROL INCLUDING ALL SIGNAGE, DAILY INSPECTION, AND MAINTENANCE – WOOD CREEK BRIDGE

OPSS.MUNI 706, April 2018, shall govern except as amended or extended herein.

706.01 SCOPE

Section 706.01 of OPSS.MUNI 706, April 2018, is amended by the addition of the following: One lane of traffic shall be maintained at all times with bi-directional traffic unless otherwise noted. One full road closure for 24 hours is permitted, refer to the drawings for details.

Work under this tender item shall include, but not be limited to the following:

- Preparation and submission of a Traffic Control Plan (TCP) as specified elsewhere in the Contract Documents;
- Supply, installation, monitoring, operation, maintenance and removal of all temporary construction signage for both vehicular and pedestrian traffic;
- Coordination and execution of all traffic control activities required to complete the work;
- All other provisions related to traffic control and traffic management as required to carry out the construction and facilitate the Contractor's operations;
- Removal, salvage and reinstallation of all existing roadway and information signage within the Contractor's work area.
- Supply, placement, relocation and removal of steel road plates and fasteners required to cover removal areas.
- Supply of steel road plates and fasteners required to cover joint for emergency re-opening of road.
- Supply, placement and removal of cold patch material required to smooth transition from existing roadway surfaces to steel road plates.
- Supply, surface preparation and placement of WaboCrete® II patching material to repair holes in deck and approach slab created to anchor steel road plates to each element.

The Contractor shall be responsible for construction means, methods, techniques, sequences, procedures, coordination and maintenance of all requirements for traffic signing.

706.02 REFERENCES

Section 706.02 of OPSS.MUNI 706, April 2018, is amended by the addition of the following:

Ontario Traffic Manual (OTM): All reference in this Special Provision and the Contract Documents to the Ontario Traffic Manual will indicate the most recent version(s) of the OTM

including, and not necessarily be limited to, as applicable:

- Book 1 – Introduction of the Ontario Traffic Manuals
- Book 5 – Regulatory Signs
- Book 6 – Warning Signs
- Book 7 – Temporary Conditions
- Book 11 – Markings and Delineations
- Book 12 – Traffic Signals.

706.03 DEFINITIONS

Section 706.03 of OPSS.MUNI 706, April 2018, is amended by the addition of the following:

Traffic Control Device(s) (TCD)

A generic term used to describe any person, sign, signal, marking or device placed upon, over or adjacent to a roadway by or at the direction of a public authority or official having jurisdiction (such as the Township of South Glengarry) or their designate, for the purpose of regulating, warning, guiding or informing a vehicle operator or pedestrian of an existing condition or hazard.

Traffic Control Plan (TCP)

A detailed plan for the control of traffic, including vehicular and pedestrian movements, required to allow the Contractor to fulfil all conditions of the contract, taking into account the organized, systematic safe conduct of the project. This includes, as applicable, detours, staging sequences, work, public and emergency vehicle access and egress, public access and separation from hazardous areas, temporary barriers, removal of old pavement markings, and the selection of appropriate typical layouts and devices necessary for traffic control. The traffic control plan shall contain the information respecting how the applicant intends to complete the work and satisfy the requirements of the Contract including but not limited to the following:

- a) start and completion times of work;
- b) specific location of work;
- c) lane use requirements;
- d) requirements for road closure;
- e) public notification undertaken;
- f) requirement for temporary no stopping signs;
- g) traffic routing and detour requirements where required

Traffic Control Persons (TCP's): A person duly trained and authorized to direct traffic at a work zone through the use of the Traffic Control Sign (STOP/SLOW Paddle)

Traffic Protection Plan (TPP): A plan required by the Occupational Health and Safety Act and its regulations for the protection of workers in a work zone. The plan must contain a written description of the traffic hazards to which workers may be exposed and measures used to protect them.

OPSS.MUNI 706, April 2018, is amended by the addition of Section 706.04 as follows:

706.04 SUBMISSION AND DESIGN REQUIREMENTS

706.04.01 General Requirements

The Ontario Traffic Manual supersedes all references to the Ministry of Transportation, Manual for Uniform Traffic Control Devices, or MUTCD in the Contract Documents.

The Contract Administrator reserves the right to ask for revisions to the Traffic Control Plan at submission time, or reject it if the Plan does not meet the Contract language. In addition, the Contract Administrator reserves the right to instruct the Contractor to revise it at any time during the Contractor's execution of the plan, when the Contract Administrator finds that the Contractor is not providing the commitments shown in the original Traffic Control Plan submission, or the Contractor's Traffic Control Plan proves to be insufficient to address the field conditions.

The Contractor shall be required to review and modify the TCP for errors, omissions, deficiencies,

or because of any new hazards that are identified and not previously addressed within the document.

It is the responsibility of the Contractor to ensure that all necessary training has been provided prior to commencement of the work.

The Contractor shall ensure that all workers, including sub-contractors, in the Working Area are aware of the importance of the Traffic Control Plan measures.

706.04.02 Submission Requirements

Two (2) weeks prior to commencing construction, the Contractor shall prepare and submit to the Contract Administrator a Traffic Control Plan that details the specific traffic control layout(s) necessary for the completion of the works. The Traffic Control Plan shall be in the form of drawing(s) and written description(s) of how the Contractor intends to control traffic through and around the work zone. The TCP shall include, and not necessarily be limited to:

- Monitoring and Repair (24 hour contact number);
- Reference to Applicable OTM Book 7 Typical Layouts;
- Traffic control signs (regulatory, warning and temporary);
- Traffic control delineation;
- Traffic Control vehicles and devices (TC-12, Crash Trucks, Temporary Lighting etc.);
- Contract-specific operational requirements;
- Traffic staging and scheduling;
- Construction vehicle access/egress;
- Public access/egress for all existing entrances and side roads;
- Pedestrian safety; barriers and barricades;
- Emergency Vehicle access;
- Locations for removal of existing line painting and proposed temporary pavement markings;
- Parking for Contract Administrator; and
- Any other traffic control measures.

The Contract Administrator will accept the submission of the TCP, and review it to identify any errors, omissions, or improvements as it relates to maintaining public safety and mobility. The acceptance and review of the TCP by the Contract Administrator will make no representation and/or warranty that the document is accurate, complete, or compliant with all applicable legislation. Any errors, omissions or deficiencies within the TCP will remain the sole responsibility of the Contractor. Work shall not commence until the Contract Administrator has reviewed the TCP and the Contractor has addressed all comments.

706.07 CONSTRUCTION

Section 706.07 of OPSS.MUNI 706, April 2018, is amended by the addition of the following:

706.07.04 Additional Requirements

Prior to commencing any construction, on or adjacent to a Township Road, the supply and placement of all necessary temporary traffic control devices shall be performed under the sole direction of the Contractor and in accordance with the Contractors submitted/reviewed TCP. The TCP shall be developed in accordance with guidelines established by the most recent version of the OTM, the Occupational Health and Safety Act (OHSA), and the Contract Documents which details the required contents and submission of the TCP.

The Contractor shall designate a person to be responsible for traffic control and work zone safety. The designated person shall be a competent worker who is qualified because of knowledge, training, and experience to perform the duties; is familiar with Book 7 of the OTM; and has knowledge of all potential or actual danger to workers and motorists. Prior to the commencement

of construction, the Contractor shall notify the Contract Administrator of the name; address; position and telephone numbers of the designated person, and update as necessary. The designated person may have other responsibilities, including other construction sites, and need not be present in the Working Area at all times.

The condition of all traffic control devices shall be maintained for the duration of the Contract, in accordance with the OTM.

The Contractor shall immediately repair, replace or otherwise make good the practice deemed unsafe or non-compliant when the Owner (or his delegated authority) makes the Contractor aware of any violation of the TCP (or applicable regulations). Should the Contractor disagree, the Ministry of Labour will be consulted to provide clarification of the observed deficiency.

Vehicular and pedestrian traffic control shall remain the sole responsibility of the Contractor as the Township delegates this authority to the Contractor in accordance with the submitted/reviewed TCP. Notwithstanding the foregoing, the Contractor shall, at his own expense, remove any equipment or material, which in the Contract Administrator's opinion constitutes a hazard to traffic or pedestrians.

The Contractor shall be fully and solely responsible to ensure the development and implementation of a submitted/reviewed traffic control plan as specified elsewhere in the Contract. The TCP and all required traffic control devices shall be designed/installed, monitored, operated/maintained and removed by the Contractor, utilizing only competent persons and workers as defined under the OHSA.

The Contractor shall not store any equipment or materials on the road or the roadway shoulders or boulevards, unless the storage areas are identified in the TCP and appropriate traffic control devices protect the equipment or materials. The Contract Administrator shall review and approve any storage of equipment and /or materials within the right of way.

The Contractor shall remove all dirt and debris from all paved or concrete surfaces at the close of each workday, to the satisfaction of the Contract Administrator.

Vehicle Access to Entrances and Side Roads

The Contractor shall not block access to private entrances. Should temporary loss of access/egress be necessary to complete the work outlined in the Contract, the Contractor must provide a minimum of 24 hr prior notification to the business or resident(s), notifying them of the work being undertaken. Such notification regarding the temporary loss of access/egress is the sole responsibility of the Contractor.

This requirement will not be considered for separate payment.

Signage

The Contractor shall be responsible for all aspects of traffic control during construction. This includes the removal, installation or modification of any necessary regulatory signage due to the requirements of the traffic control plan and the supply, installation, maintenance and removal of all temporary regulatory/construction signage.

The Contractor shall not make any claim for extra compensation for the cost of fulfilling the obligations set out in this Special Provision.

706.10 BASIS OF PAYMENT

706.10.01 Traffic Control Signage

Subsection 706.10.01 of OPSS.MUNI 706, April 2018, is deleted in its entirety and replaced by the following:

Payment at the Contract price for the above item shall be full compensation for all labour, equipment and material required to do the work. All research, preparation, implementation of the TCP, supply, installation, monitoring, operation, maintenance and removal of required traffic control devices are deemed to be included with the item.

Payment shall be based upon the following schedule:

- a) 25% upon satisfactory submission of the TCP and installation of the control measures; and
- b) 75% pro-rated into equal payments over the term of the contract.

This payment schedule may only be modified as agreed upon in writing between the Contractor and the Contract Administrator.

SP16 – JOINT SEAL – WOOD CREEK BRIDGE

OPSS.MUNI 920, November 2023, shall govern except as amended or extended herein.

906.01 SCOPE

Section 920.01 of OPSS 920 is amended by the addition of the following:

The joint seal shall be Wabo®Evazote UV – EVAZOTE 30605, manufactured by Watson Bowman Acme, or approved equivalent. All materials shall be supplied, handled, and stored according to the manufacturer's instructions.

The joint shall be properly prepared in accordance with the manufacturer's recommendations, including cleaning, surface preparation, and installation.

A representative of the manufacturer shall be on-site during installation.

The manufacturer's representative shall provide written sign-off confirming that the surface preparation and seal installation have been completed in accordance with product requirements.

SP17 – ABRASIVE BLAST CLEANING – WOOD CREEK BRIDGE

OPSS.MUNI 929, November 2018, shall govern except as amended or extended herein.

906.01 SCOPE

Section 929.01 of OPSS 929 is amended by the addition of the following:

This item shall consist of the abrasive blast cleaning of exposed reinforcing steel and preparation of concrete substrates to receive patching, bonding agents, or other repair materials.

Concrete Substrates:

All concrete surfaces exposed for patching or repair shall be abrasive blasted to remove laitances, laitance, contaminants, and coatings which may interfere with bond.

Steel Reinforcing:

Exposed reinforcing steel shall be abrasive blasted to SP10, near white, immediately prior to installation of patch materials.

Galvanized steel reinforcement shall be cleaned with Xylene or a wire wheel brush to remove oils, grease, and contaminants before patch placement.

Sequence:

Concrete substrate preparation and steel cleaning shall be completed immediately prior to placement of patching material.

No patch material shall be applied until surfaces have been inspected and approved.

SP18 – CONCRETE REMOVALS – WOOD CREEK BRIDGE

OPSS.MUNI 928, November 2021, shall govern except as amended or extended herein.

906.01 SCOPE

Section 928.01 of OPSS 928 is amended by the addition of the following:

All Concrete removals on the Wood Creek Bridge shall be paid under Item "Concrete Removals – Partial Depth – Type A". The removals shall be to the size and tolerance listed on the Contract Drawings. Additional removals beyond the specified dimensions (over-removal) and the

associated placement of patch material in those areas shall be entirely at the contractor's expense.

SP19 – TWO-COMPONENT POLYURETHANE PATCH MATERIAL WITH SPECIALTY AGGREGATE – WOOD CREEK BRIDGE

SCOPE

This item shall consist of the supply, delivery, surface preparation, placement, and curing of a two-component polyurethane patch material with specialty aggregate (WaboCrete® II) at locations shown in the Contract Documents. This item also includes supply, delivery, and placement of asphalt impregnated fiber board between the patch material and wing walls. Abrasive blast cleaning and concrete removals are paid under separate tender items.

MATERIALS

The patch material shall be WaboCrete® II, as manufactured by Watson Bowman Acme, or approved equivalent, consisting of a two-component polyurethane binder and manufacturer-supplied specialty aggregate.

The primer shall be Wabo® Bonding Agent (primer) as manufactured by Watson Bowman Acme, or approved equivalent.

All materials shall be stored, handled, and mixed in accordance with the manufacturer's written recommendations.

CONSTRUCTION

Surfaces shall be clean, dry, and properly prepared in accordance with the manufacturer's requirements.

A primer shall be applied to all prepared concrete surfaces immediately prior to placement, in accordance with the manufacturer's instructions.

The two-component polyurethane binder and specialty aggregate shall be proportioned, mixed, and placed in strict accordance with the manufacturer's written instructions.

The material shall be placed and consolidated to ensure full bond to the substrate and encapsulation of reinforcement. The finished surface shall be struck off to the required grade and profile.

Curing shall be carried out in accordance with the manufacturer's recommendations. The patch material shall not be opened to traffic until the minimum curing time has been achieved.

If a joint between the deck end and approach slab below the limits of removals is present the contractor shall seal the gap with silicon sealer or backing rod to prevent epoxy leakage.

SUBMITTALS / WORK PLAN

At least 5 Working Days prior to the full road closure, the Contractor shall submit a Work Plan to the Contract Administrator for review.

The Work Plan shall include, at a minimum:

- A detailed hourly schedule of work activities for the duration of the closure.
- Worker roles and responsibilities, including crew size and task assignments.
- Sequence of operations for surface preparation, priming, placement, curing, and reopening to traffic.
- Contingency measures for delays or emergency reopening of the roadway.

No full road closure shall proceed until the Work Plan has been reviewed and accepted by the Contract Administrator.

QUALITY ASSURANCE / MANUFACTURER SIGN-OFF

The manufacturer's representative shall be present on-site and shall provide written sign-off prior to proceeding to the next stage of work, including:

1. Surface preparation
2. Placement of patch material
3. Curing

The sign-off shall be submitted to the Contract Administrator and acknowledged before the Contractor may proceed to subsequent work steps.

MEASUREMENT FOR PAYMENT

Measurement shall be by each, based on the number of units of material supplied and incorporated into the Work.

One unit shall be defined as 0.6 cubic feet (0.01699 m³) of WaboCrete® II material.

BASIS OF PAYMENT

Payment at the Contract price per each for this item shall be full compensation for all labour, equipment, and materials required for the supply, delivery, surface preparation, placement, and curing of the patch material. The supply, delivery, and installation of asphalt impregnated fiberboard is included in this item.

No additional payment shall be made for waste, or material placed beyond the limits shown on the Contract Drawings.

Contract Drawings: The Contract drawings are amended by addition of the following:

United Counties of Stormont, Dundas and Glengarry **ROADS DEPARTMENT**



**Keystone Bridge
Management Inc.**

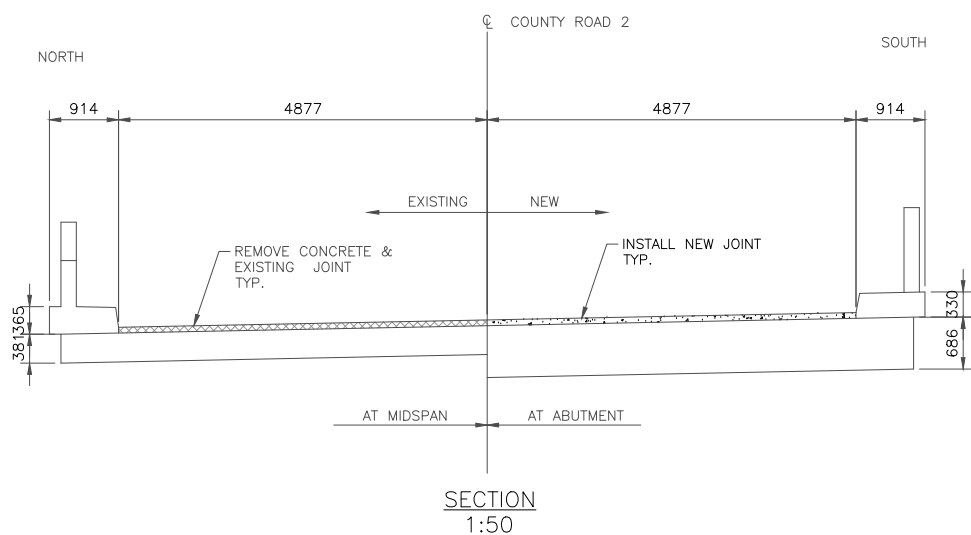
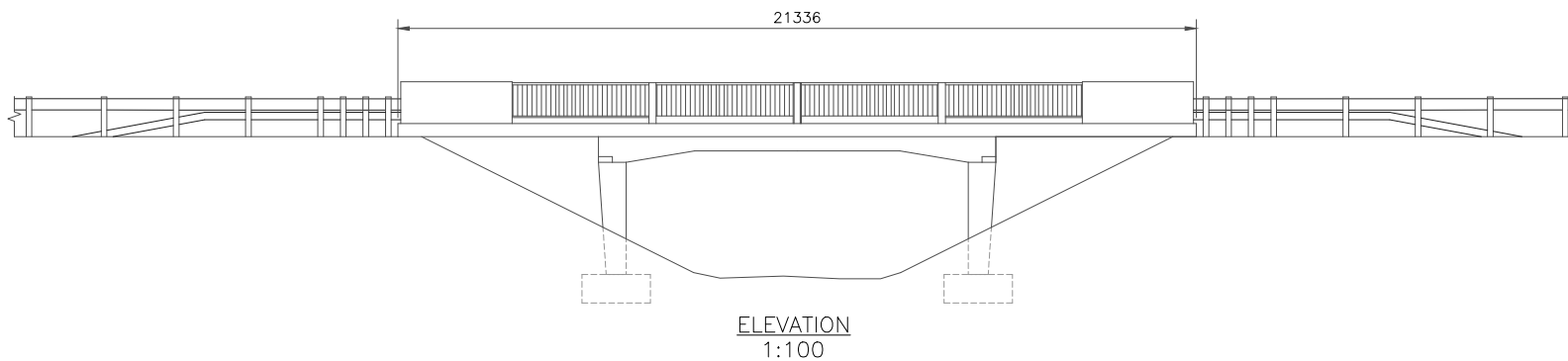
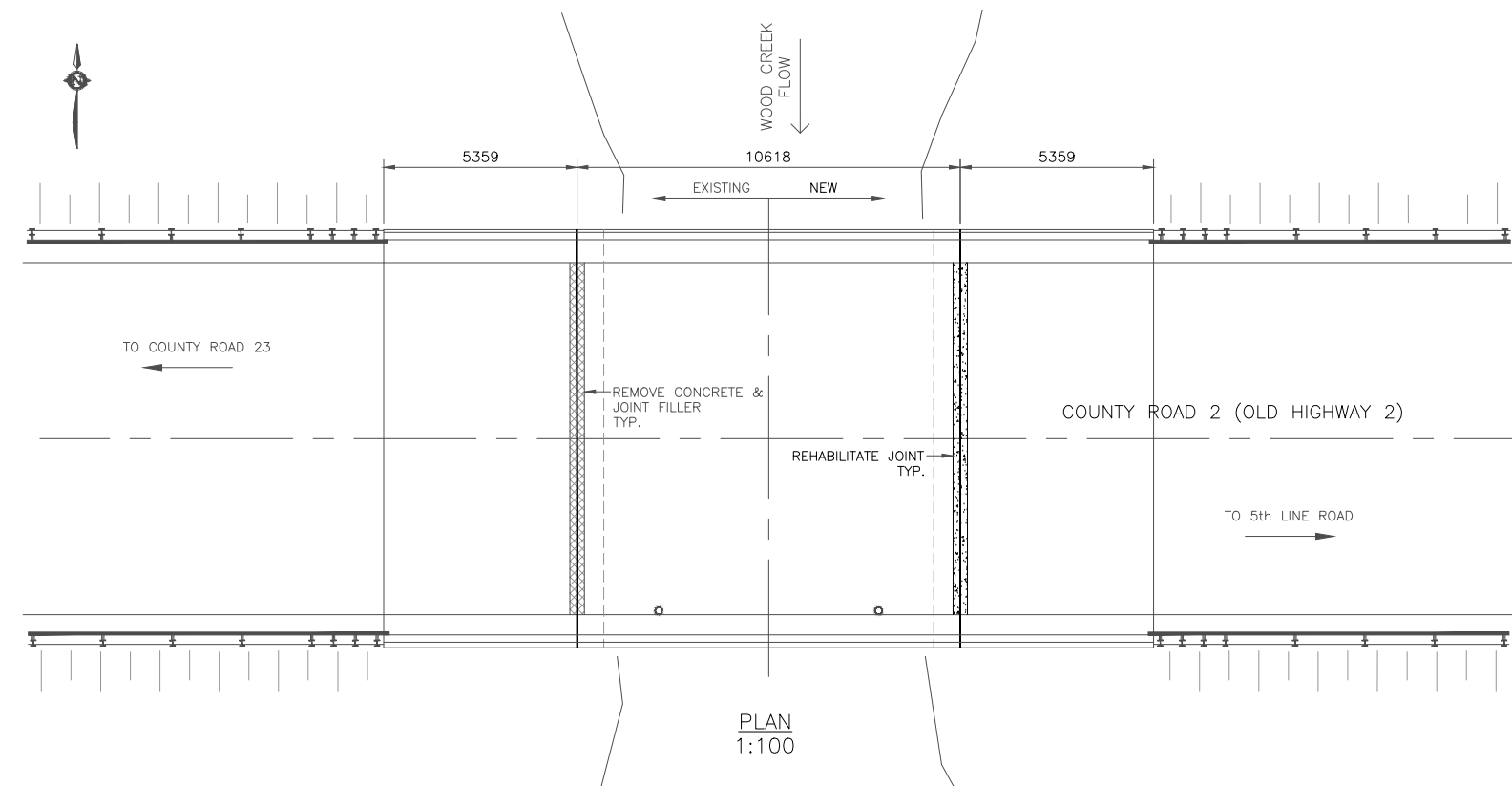


LIST OF DRAWINGS

- 1. GENERAL ARRANGEMENT
- 2. DETOUR
- 3. DETAILS

MARCH 2026

WOOD CREEK BRIDGE JOINT REPAIR
CONTRACT No. 2026-04
ISSUED FOR TENDER - ADDENDUM 1



GENERAL NOTES

1. DESIGN CODE

CANADIAN HIGHWAY BRIDGE DESIGN CODE (CAN/CSA-S6-25) CL-625-ONT LOADING.

2. CLEAR COVER TO REINFORCING

DECK TOP 70 ± 20
SOFFIT 40 ± 10
REMAINDER SHALL BE 70 ± 20 UNLESS OTHERWISE NOTED

3. REINFORCING STEEL

REINFORCING STEEL SHALL BE GRADE 400W UNLESS OTHERWISE SPECIFIED.

UNLESS SHOWN OTHERWISE, TENSION LAP SPLICES FOR REINFORCING STEEL BARS SHALL BE CLASS B.

STAINLESS REINFORCING STEEL SHALL BE TYPE 316LN OR DUPLEX 2205 AND HAVE A MINIMUM YIELD STRENGTH OF 500 MPa. UNLESS OTHERWISE SPECIFIED.

BAR MARKS WITH PREFIX 'S' DENOTE STAINLESS STEEL BARS. BAR HOOKS SHALL HAVE STANDARD HOOK DIMENSIONS USING MINIMUM BEND DIAMETERS, WHILE STIRRUPS AND TIES SHALL HAVE MINIMUM HOOK DIMENSIONS. ALL HOOKS SHALL BE IN ACCORDANCE WITH THE STRUCTURAL STANDARD DRAWING SS112-1, UNLESS INDICATED OTHERWISE.

BAR HOOKS SHALL HAVE STANDARD HOOK DIMENSIONS USING MINIMUM BEND DIAMETERS, WHILE STIRRUPS AND TIES SHALL HAVE MINIMUM HOOK DIMENSIONS. ALL HOOKS SHALL BE IN ACCORDANCE WITH THE STRUCTURAL STANDARD DRAWING SS112-1 UNLESS INDICATED OTHERWISE.

4. CONSTRUCTION NOTES

THE CONTRACTOR SHALL VERIFY ALL EXISTING DIMENSIONS, DETAILS AND ELEVATIONS THAT ARE RELEVANT TO THE WORK SHOWN ON THE DRAWINGS PRIOR TO COMMENCEMENT OF THE WORK AND FABRICATION OF REINFORCING STEEL. ANY DISCREPANCIES SHALL BE REPORTED TO THE CONTRACT ADMINISTRATOR AND THE PROPOSED ADJUSTMENT OF THE WORK REQUIRED TO SHALL BE SUBMITTED FOR APPROVAL.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING THE STRUCTURAL STABILITY AND INTEGRITY OF THE STRUCTURE AT ALL STAGES OF CONSTRUCTION.

ALL EXPOSED PATCH MATERIAL EDGES SHALL HAVE A 10 mm X 10 mm CHAMFER UNLESS NOTED OTHERWISE.

SUPPORTS FOR REINFORCING STEEL SHALL BE AS PER OPSD 3329.101 AND OPSD 3329.100 ON FORMED SURFACES. ON NON-FORMED SURFACES, CONCRETE BLOCKS (MIN 20MPA) SHALL BE USED.

ALL MATERIALS SHALL BE IN ACCORDANCE WITH MTO DESIGNATED SOURCE MATERIALS (DSM).

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL LOCATES AND PROTECTION OF ALL UTILITIES DURING CONSTRUCTION OPERATIONS.

ALL EXISTING CONCRETE SURFACES IN CONTACT WITH NEW PATCH MATERIAL SHALL BE UNIFORMLY ROUGHENED TO A SURFACE PROFILE OF 5 mm ± 2 mm BY MEANS OF METHODS SUCH AS SCABBLING, CHIPPING, OR BUSH HAMMERING TO EXPOSE THE AGGREGATES ACROSS THE ENTIRE SURFACE.

THE CONTRACTOR SHALL PROTECT ALL EXISTING SIGNAGE FROM DAMAGE WITHIN CONTRACT LIMITS, AND SALVAGE, STORE AND REINSTATE EXISTING SIGNAGE WHERE REQUIRED.

DOWEL EMBEDMENT SHALL BE 150 mm.

SAWCUTS AT LIMITS OF REMOVALS SHALL BE 25 mm DEEP OR TO THE FIRST LAYER OF REINFORCING STEEL, WHICHEVER IS LESS.

PATCH MATERIAL SHALL BE WABOCRETE® II MANUFACTURED BY WATSON BOWMAN ACME A SIKA COMPANY.

COMPRESSION SEAL SHALL BE WABO®EVAZOTE UV - EVAZOTE 30605 MANUFACTURED BY WATSON BOWMAN ACME A SIKA COMPANY.

DOWELS AND REINFORCING STEEL SHALL BE INSTALLED PRIOR TO ABRASIVE BLAST CLEANING. THREE EXTRA STRAIGHT DOWELS SHALL BE SUPPLIED FOR PULL TESTING AND INSTALLED AT THE LOCATION DIRECTED BY THE CONTRACT ADMINISTRATOR AND SHALL BE CUT OFF AFTER PULL TESTS ARE COMPLETE.

STAINLESS STEEL BARS SHALL BE SEPARATED FROM GALVANIZED STEEL MESH IN THE OVERLAY BY PLASTIC SPACERS OR PLASTIC TAPE.

ABRASIVE BLAST CLEANING OF EXISTING REINFORCING STEEL AND ROUGHENING OF CONCRETE SURFACES SHALL BE CARRIED OUT IN ACCORDANCE WITH BOTH OPSS.MUNI 929 AND PATCH MATERIAL MANUFACTURES SURFACE PREPARATION REQUIREMENTS.

ABRASIVE BLAST CLEANING SHALL BE PERFORMED DURING THE FULL ROAD CLOSURE.

CONCRETE SHALL CLEAN AND DRY PRIOR TO APPLICATION OF BONDING AGENT (PRIMER) ON CONCRETE SURFACES. BONDING AGENT (PRIMER) SHALL BE APPLIED IN ACCORDANCE WITH PATCH MATERIAL MANUFACTURER RECOMMENDATIONS.

CONCRETE REMOVALS SHALL BE LIMITED TO THE DIMENSIONS SHOWN ON THE DRAWINGS. ANY ADDITIONAL REMOVALS BEYOND THE SPECIFIED DIMENSIONS (OVER-REMOVAL) AND THE ASSOCIATED PLACEMENT OF PATCH MATERIAL IN THOSE AREAS SHALL BE ENTIRELY AT THE CONTRACTOR'S EXPENSE.

PATCH MATERIAL SHALL BE ALLOWED TO CURE PER MANUFACTURER RECOMMENDATIONS BEFORE SUBJECTING TO TRAFFIC OR INSTALLING THE COMPRESSION SEAL.

THE COMPRESSION SEAL SHALL BE PLACED 10 MM BELOW THE TOP SURFACE OF THE PATCHING MATERIAL.

HOLES IN THE DECK AND APPROACH SLAB CREATED TO FASTEN THE STEEL ROAD PLATES DOWN SHALL BE CLEANED AND FILLED WITH WABOCRETE II.

THE MANUFACTURER'S REPRESENTATIVE FOR THE PATCH MATERIAL AND COMPRESSION SEAL SHALL BE ON SITE TO CONFIRM AND PROVIDE WRITTEN APPROVAL TO THE CONTRACT ADMINISTRATOR THAT THE FOLLOWING WORK COMPLIES WITH PRODUCT REQUIREMENTS: SURFACE PREPARATION, PRIMER APPLICATION, PLACEMENT OF PATCH MATERIAL, JOINT SURFACE PREPARATION, AND COMPRESSION SEAL INSTALLATION.

RECOMMENDED SEQUENCE OF WORK:

THE FOLLOWING WORK SHALL BE CARRIED OUT IN STAGES AND IS NOT INTENDED TO BE AN EXHAUSTIVE LIST OF ALL ITEMS REQUIRED TO COMPLETE THE REHABILITATION WORKS.

DAY 1

1. DIVERT TRAFFIC ONTO WESTBOUND LANE, CLOSING EASTBOUND LANE UNDER TEMPORARY FLAGGING CONDITIONS.
2. COMPLETE CONCRETE REMOVALS ON JOINT AS SHOWN IN THE CONTRACT DRAWINGS AND AS DIRECTED BY THE CONTRACT ADMINISTRATOR.
3. INSTALL DOWELS AND REINFORCING STEEL.
4. PLACE ROAD PLATE OVER JOINT AND SECURE ROAD PLATE TO THE DECK AND APPROACH SLAB WITH TEMPORARY CONCRETE ANCHORS.
5. FULLY REOPEN OLD HIGHWAY 2 TO TRAFFIC.

DAY 2

6. REPEAT STEPS 1 TO 5 FLIPPING TRAFFIC ONTO EASTBOUND LANE AND CLOSING WESTBOUND LANE.

DAY 3

7. INSTALL TRAFFIC SIGNAGE AND FULLY CLOSE OLD HIGHWAY 2.
8. REMOVE TEMPORARY ROAD PLATES.
9. ABRASIVELY BLAST CLEAN EXISTING REINFORCING STEEL AND CONCRETE SURFACES.
10. FORM JOINT, PRIME CONCRETE SURFACES WITH WABO BONDING AGENT (PRIMER) AND PLACE WABOCRETE II.
11. ONCE WABOCRETE II HAS CURED, ROUGHEN SURFACE OF JOINT, CLEAN THOROUGHLY AND INSTALL WABO EVAZOTE UV COMPRESSION SEAL.
12. RE-OPEN OLD HIGHWAY 2 TO TRAFFIC AND REMOVE TRAFFIC SIGNAGE.



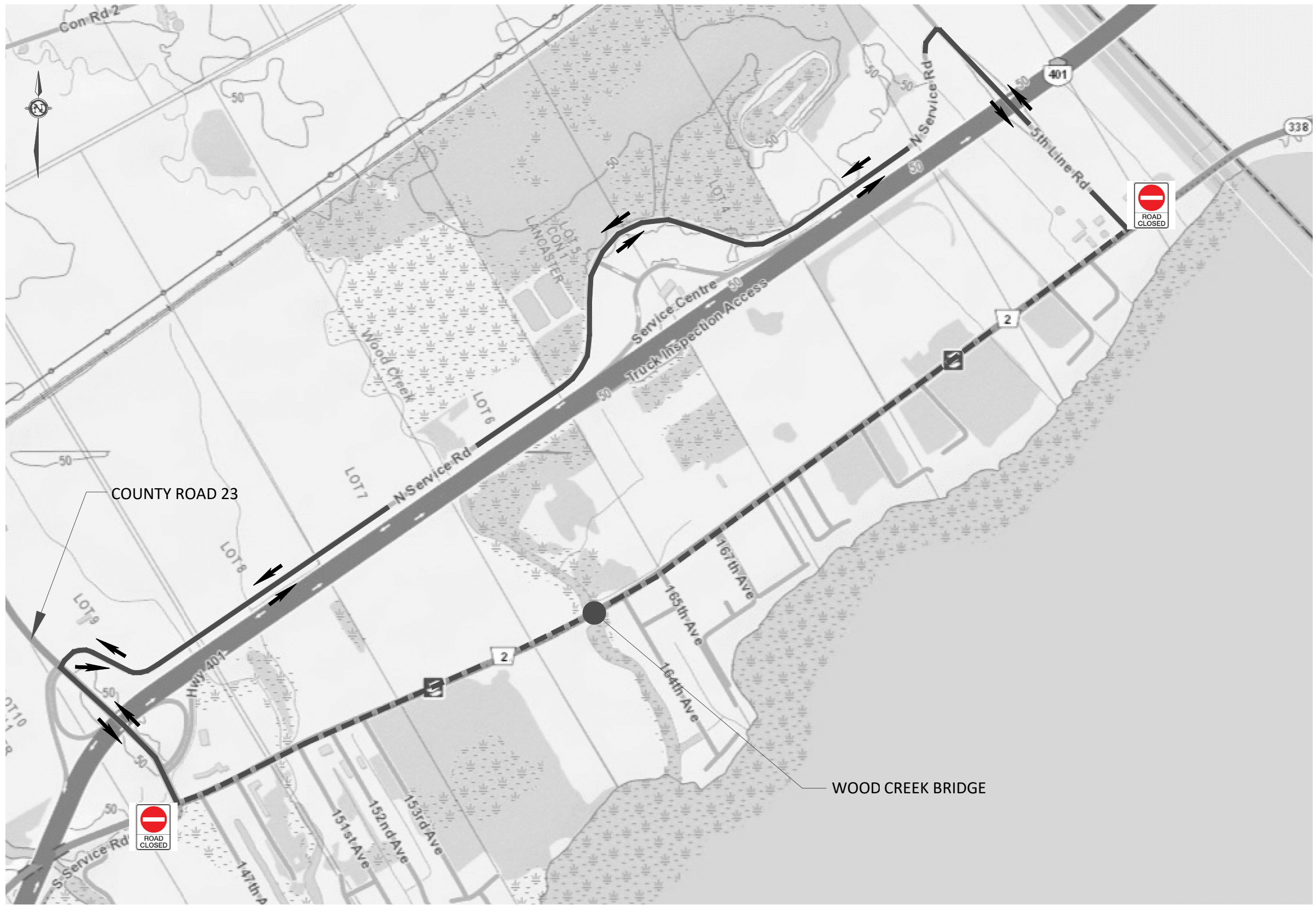
ISSUED FOR TENDER - ADDENDUM 1

Drawing No.: 1	Rev.: M. MATTHEWS
Date: MAR 2026	Drawn by: S. REID
Designed by: S. MILLER	


TRANSPORTATION & PLANNING SERVICES
 26 Pitt Street
 Cornwall, Ontario
 K6J 3P2
 P: 613-932-1515
 F: 613-936-2913


Keystone Bridge Management Inc.

02-849 WOOD CREEK BRIDGE REPAIR
 COUNTY ROAD 2
 GENERAL ARRANGEMENT



ROAD CLOSURE NOTES

1. TWO 12-HOUR WORKING DAYS OF FLAGGED TRAFFIC CONTROL ARE PERMITTED, MAINTAINING ONE BIDIRECTIONAL LANE. STEEL ROAD PLATES SHALL BE PLACED OVER REMOVAL AREAS WHEN THE LANE IS OPEN TO TRAFFIC AND SECURED TO THE APPROACH SLAB AND DECK. COUNTY ROAD 2 SHALL REMAIN FULLY OPEN TO TRAFFIC OVERNIGHT.
2. TRAFFIC BARRELS SHALL BE PLACED ALONG THE CENTERLINE OF THE BRIDGE OVERNIGHT BETWEEN WORK OPERATIONS WHEN STEEL ROAD PLATES ARE INSTALLED.
3. A 1-24-HOUR FULL ROAD CLOSURE IS PERMITTED. THE CONTRACTOR SHALL KEEP STEEL ROAD PLATES ONSITE TO ALLOW RAPID REOPENING OF COUNTY ROAD 2 IF HIGHWAY 401 IS CLOSED.
4. COLD PATCH MATERIAL SHALL BE PLACED ALONG BOTH EDGES OF STEEL ROAD PLATES AND FULLY REMOVED AND DISPOSED OF OFF-SITE UPON COMPLETION OF WORK.
5. ALL SIGNAGE TO BE SUPPLIED, INSTALLED, MAINTAINED AND REMOVED BY THE CONTRACTOR.
6. ALL SIGNAGE AND SETUP PROCEDURE SHALL BE IN COMPLIANCE WITH THE ONTARIO TRAFFIC MANUAL TEMPORARY CONDITIONS, BOOK 7. THE CONTRACTOR SHALL MAINTAIN SIGNS FOR THE DURATION OF CONSTRUCTION.
7. COUNTY ROAD 2 SHALL BE CLOSED BETWEEN COUNTY ROAD 23 AND 5th LINE ROAD.
8. CONTRACTOR TO PROVIDE ADVANCE WARNING SIGNS

TC-67'S SHALL READ
 "COUNTY ROAD 2
 WILL BE CLOSED BETWEEN
 COUNTY ROAD 23
 AND 5th LINE ROAD
 DATE TO DATE"

LEGEND

- TC 67 IMAGE IS ONLY REPRESENTATIVE
- DETOUR ROUTE
- ROAD CLOSURE
- DETOUR DIRECTION



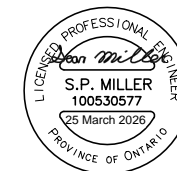
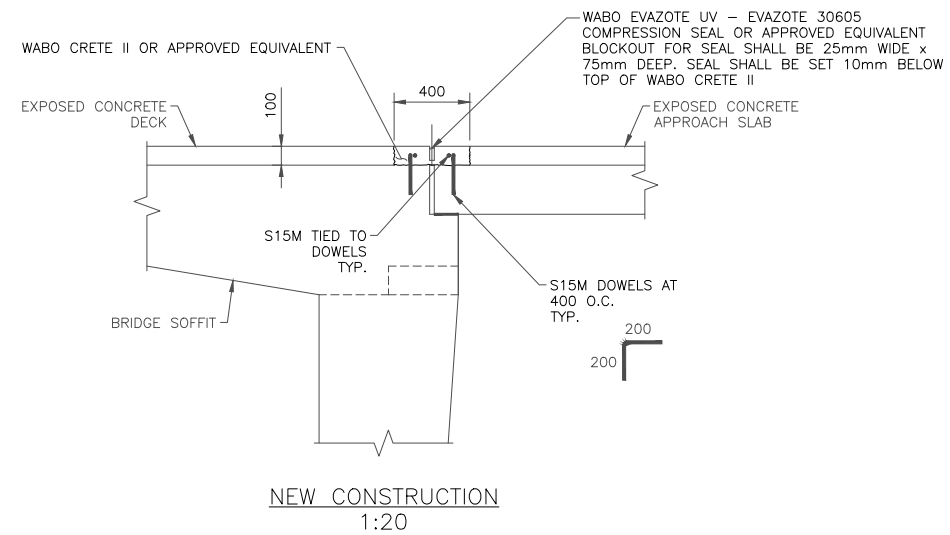
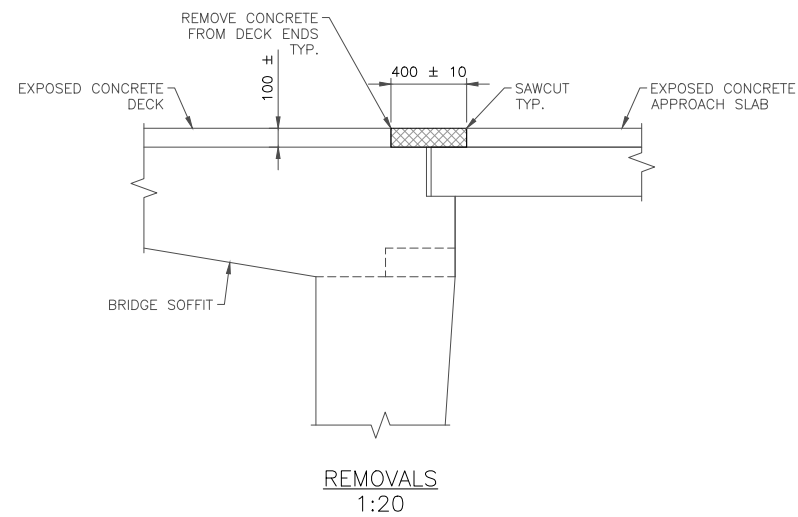
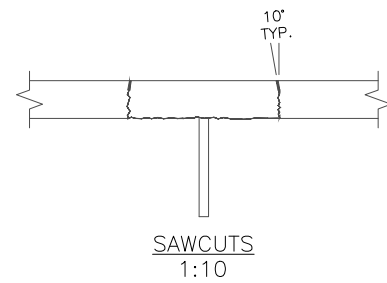
ISSUED FOR TENDER - ADDENDUM 1

Drawing No.: 2	Rev.: M. MATTHEWS
Date: MAR 2026	Drawn by: S. REID
Designed by: S. MILLER	

TRANSPORTATION & PLANNING SERVICES
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Keystone Bridge Management Inc.

02-849 WOOD CREEK BRIDGE REPAIR
 COUNTY ROAD 2
 DETOUR



ISSUED FOR TENDER - ADDENDUM 1

Drawing No.: 3

Date: MAR 2026

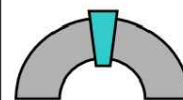
Designed by:
S. MILLER

Rev.: M. MATTHEWS

Drawn by:
S. REID



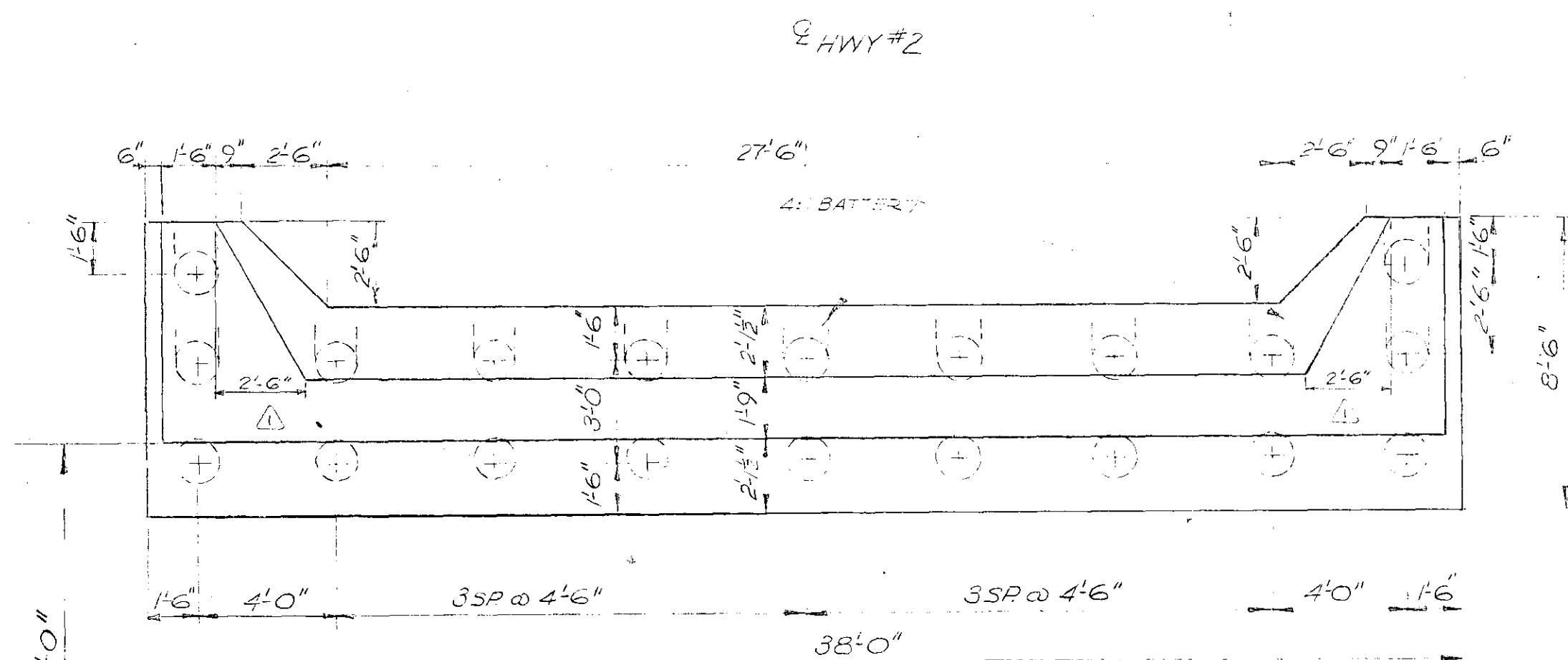
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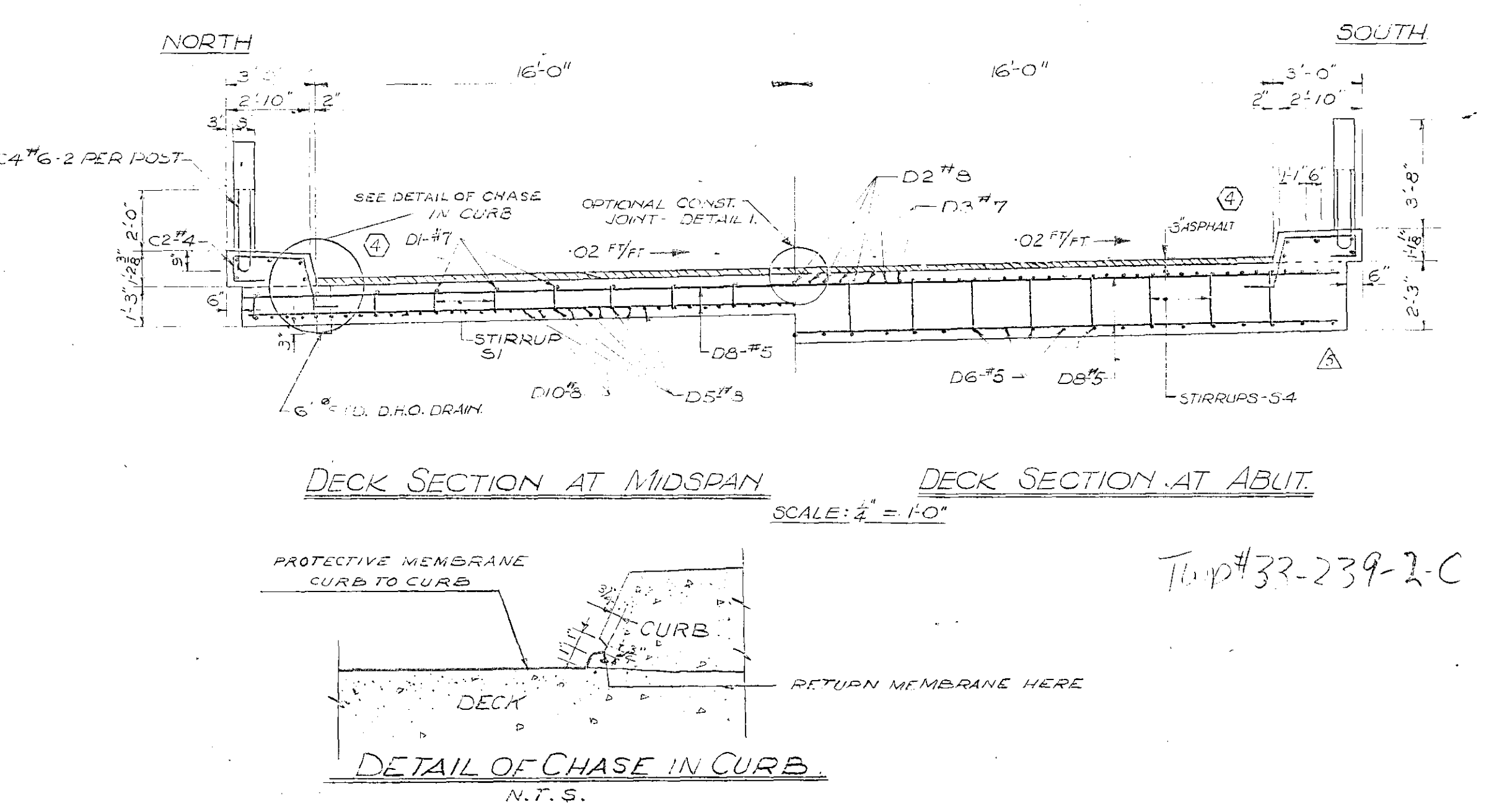
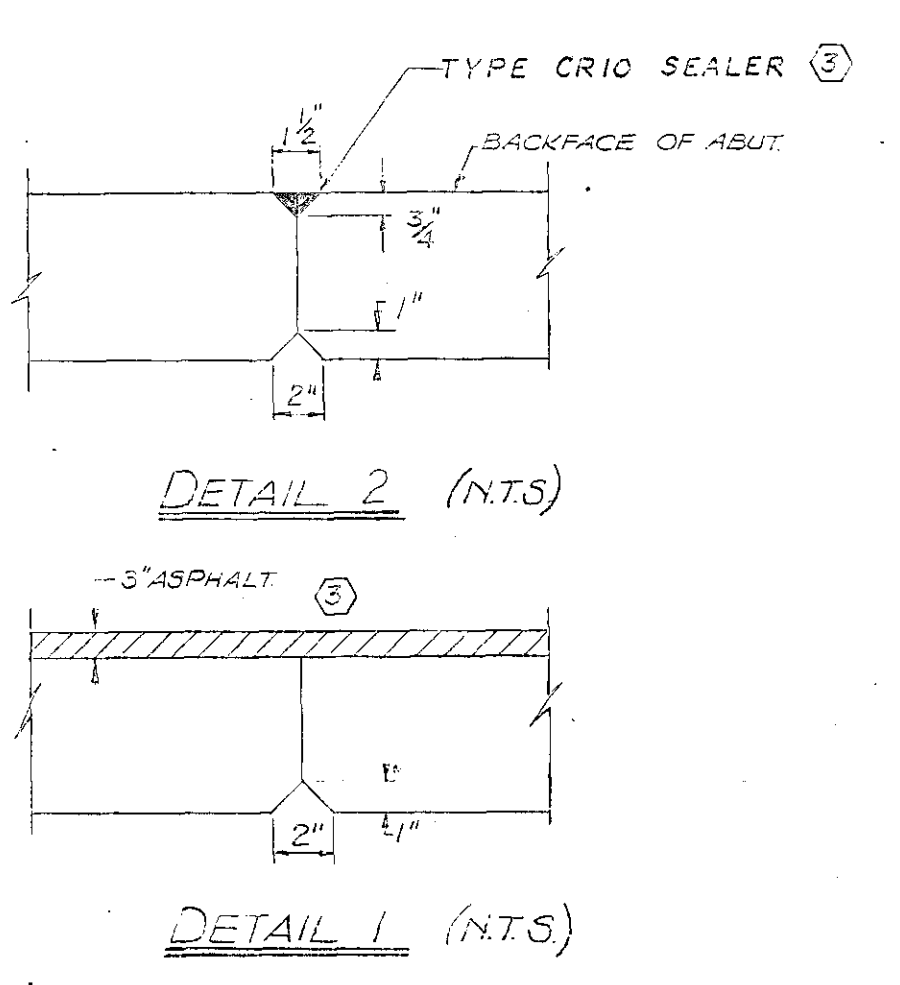
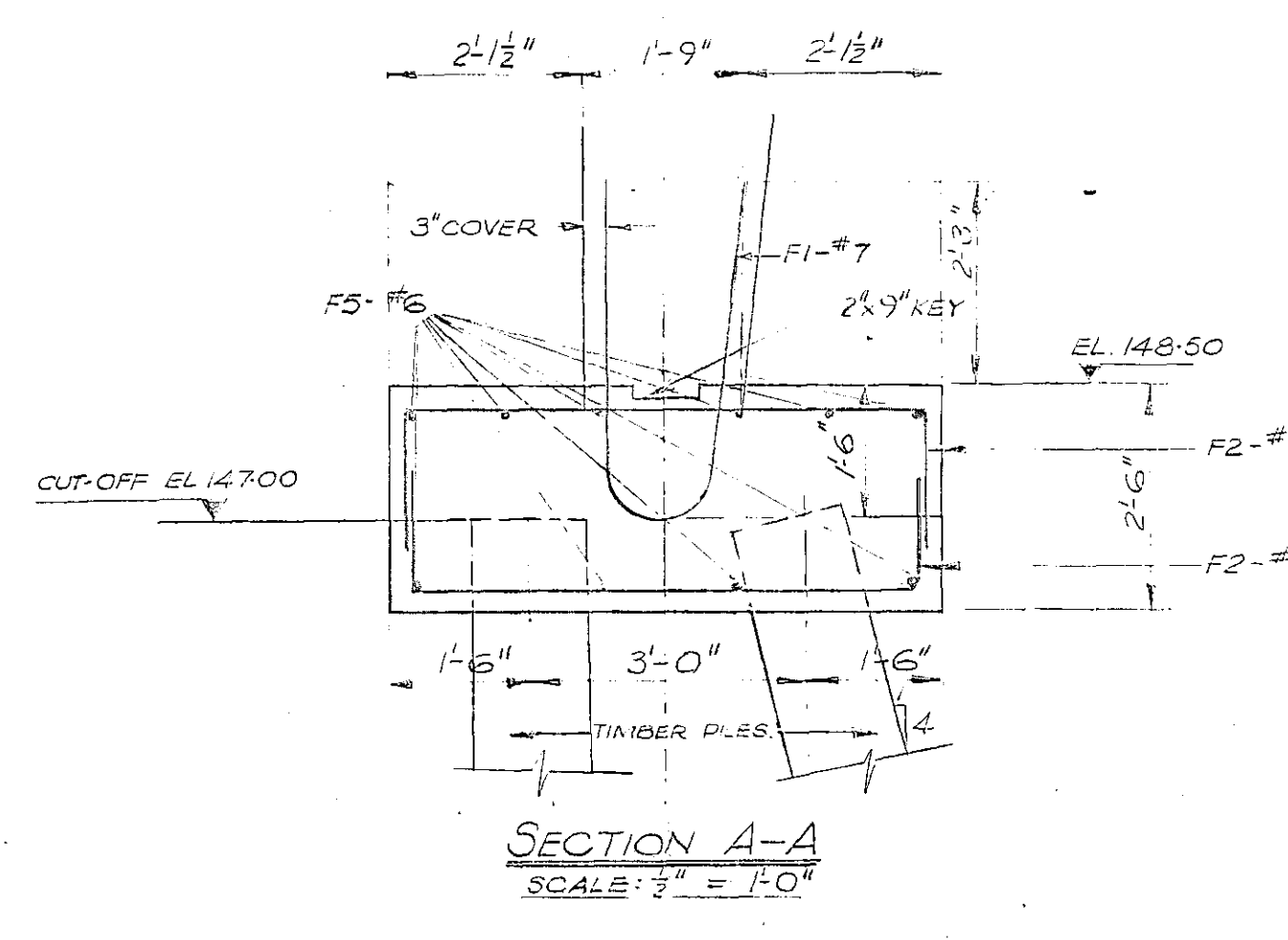
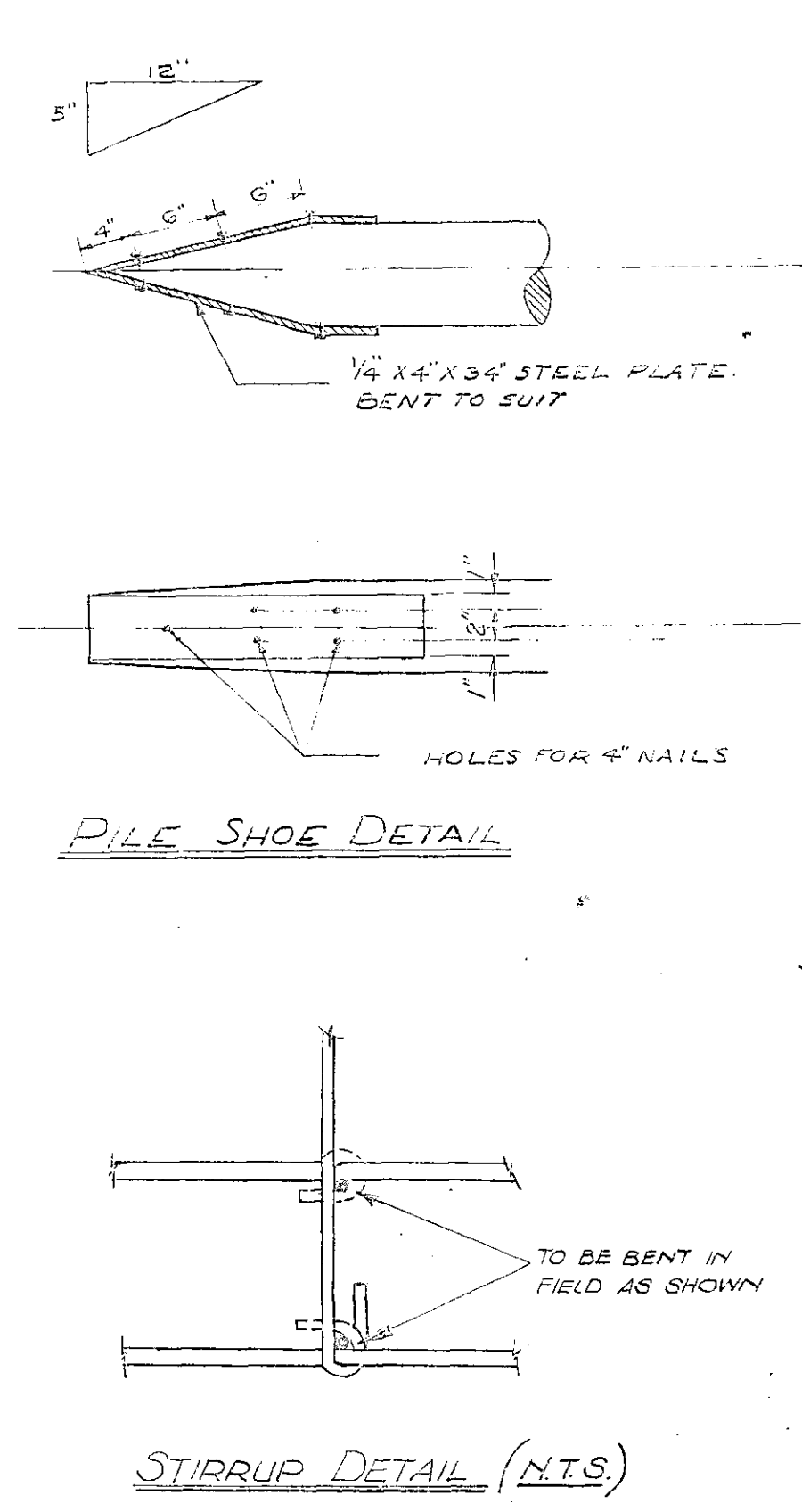
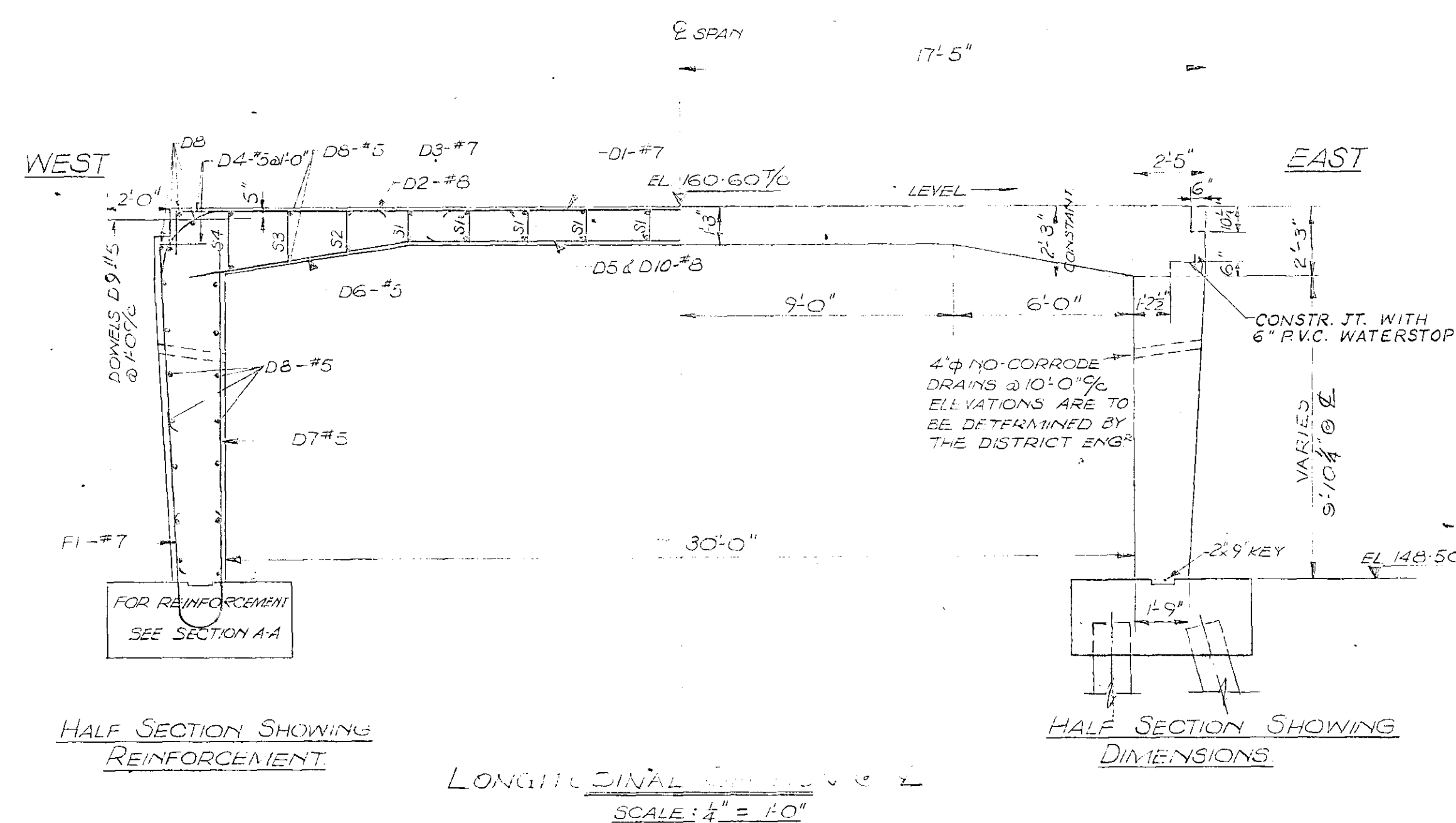
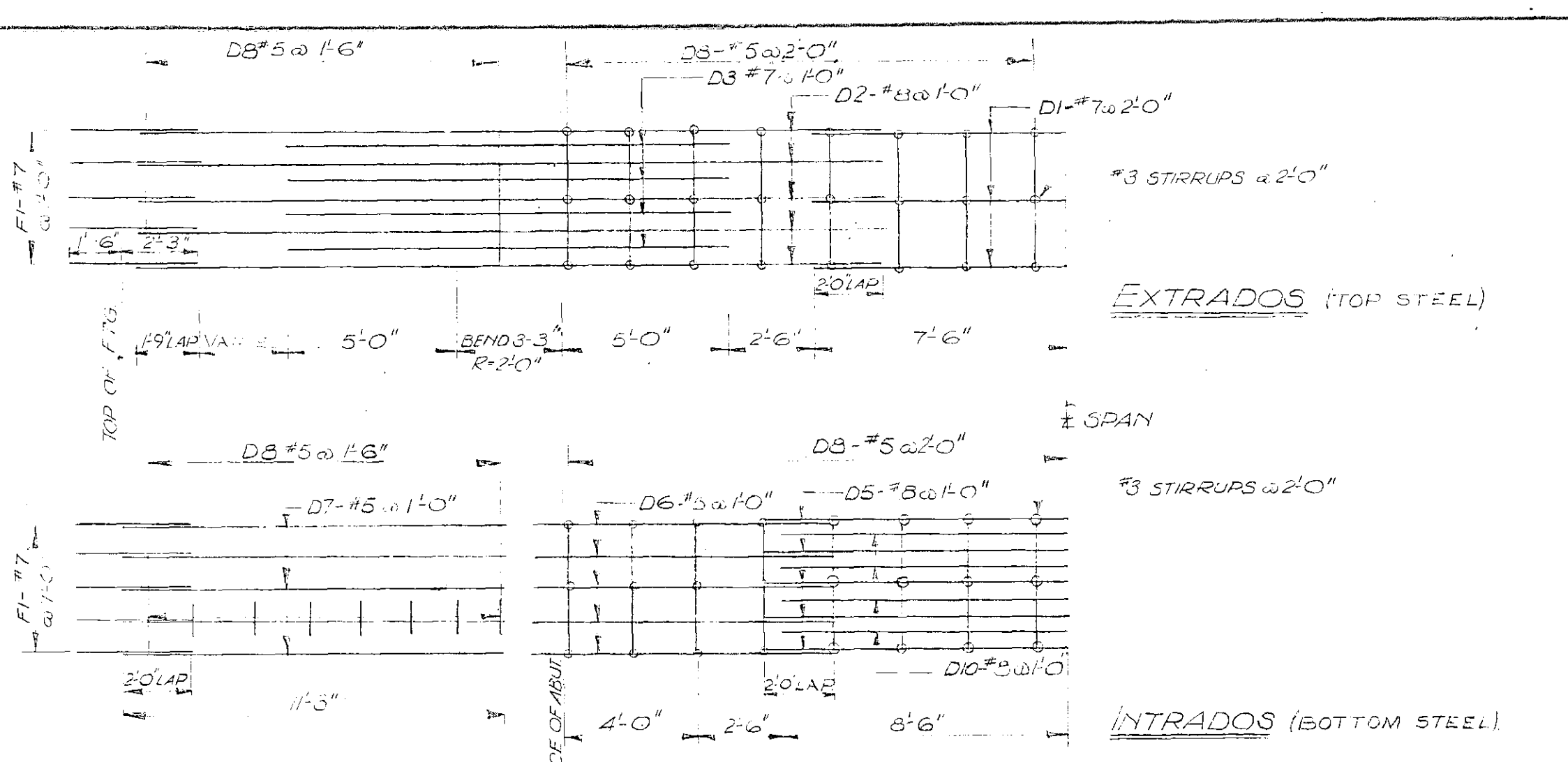
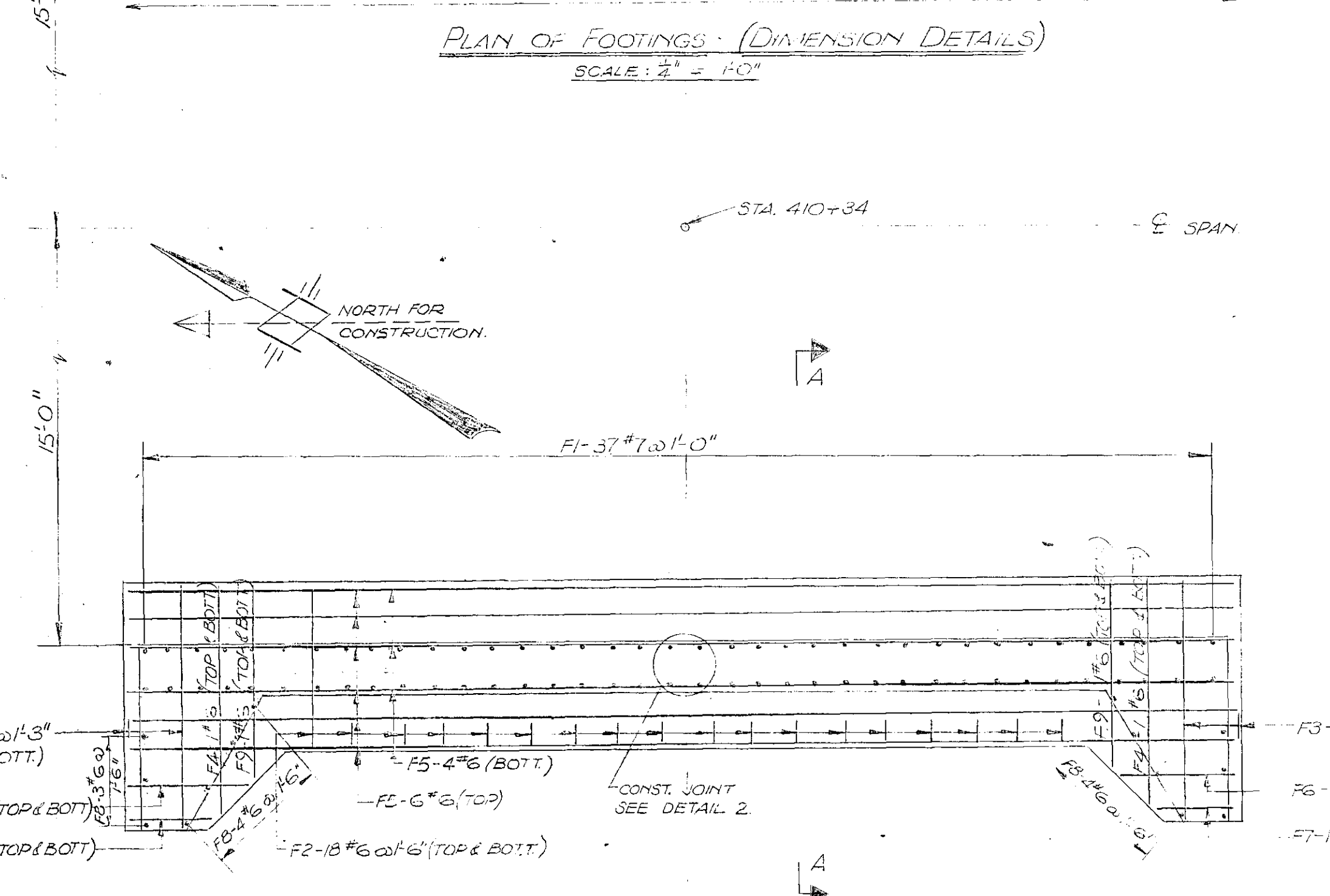
Keystone Bridge Management Inc.

02-849 WOOD CREEK BRIDGE REPAIR
COUNTY ROAD 2
DETAILS

Original Drawings: The original drawings for the Wood Creek bridges are provide below:



TYPE	TIMBER PILES
N ^o	20 / PER ABUT
LENGTH	32'-0"
DESIGN LOAD	20 TONS / PILE



REVISION	DATE	BY	DESCRIPTION
1	12-15-62	J.R.	REMOVED 3/4" DIA. DRAIN IN CURB
2	1-15-63	J.R.	REMOVED 3/4" DIA. CURB STOPS
3	5-15-63	J.R.	REMOVED PARAPLASTIC (DETAIL 1) & ADDED CRICO SEALER
4	11-22-63	J.R.	ADDED CHASE IN CURB & PILE SHOE DETAIL ADDED
5	12-7-62	J.R.	DIMENSIONS ADDED

DEPARTMENT OF HIGHWAYS ONTARIO
BRIDGE DIVISION

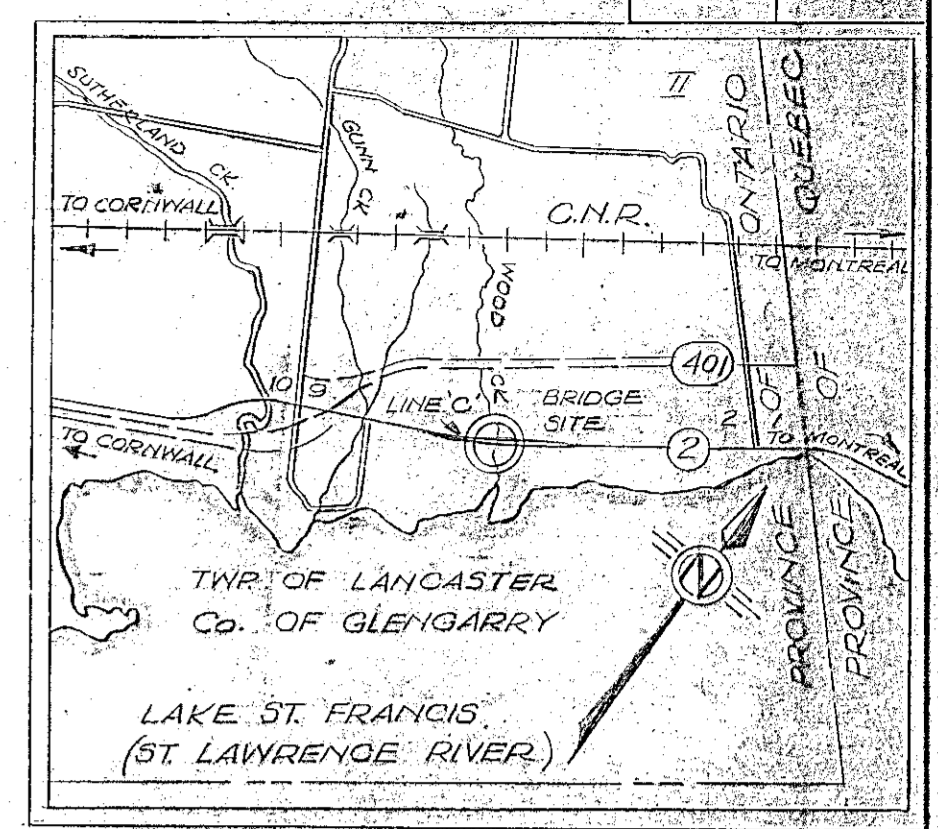
WOOD CREEK BRIDGE
(1.3M WES. OF QUEBEC BDRY)

KING'S HIGHWAY No. 2 DIST. No. 9
CO. GLENGARRY
TWP. LANCASTER LOT 6 CON. I

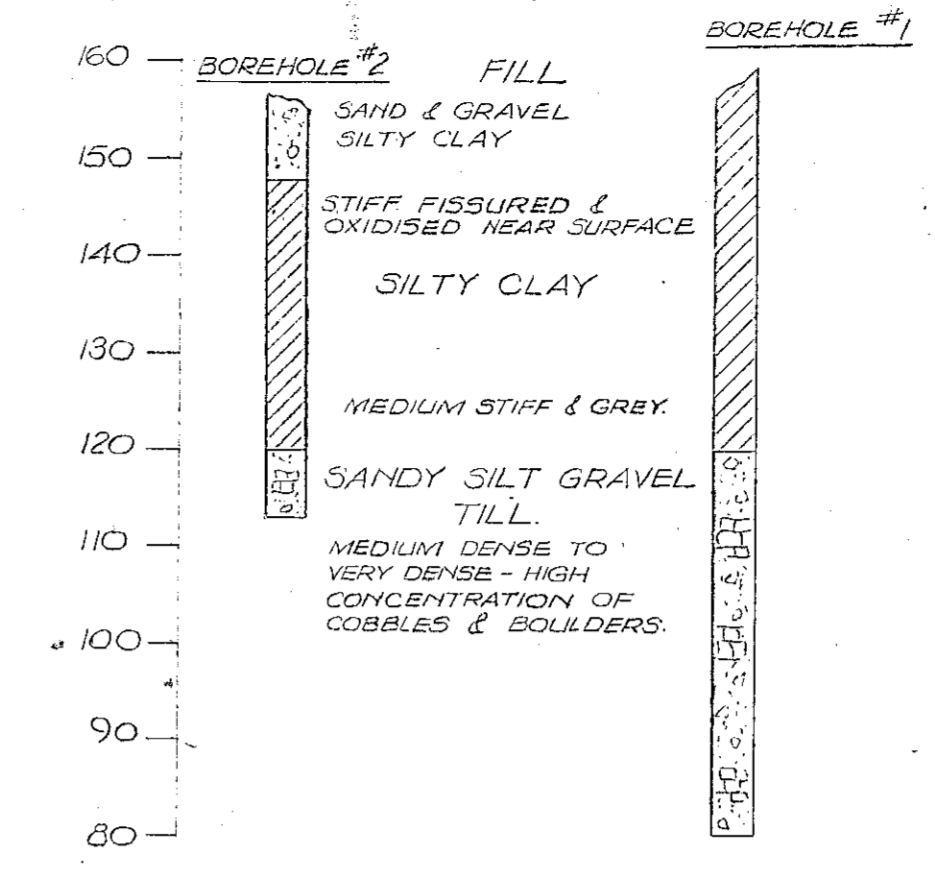
DETAILS OF FOOTINGS, ABUTMENTS & DECK.

APPROVED: [Signature] SITE No. [] V.P. No. 94-61
DESIGN: D.R.G. CHECK: J.W. CONTRACT No. []
DRAWING: J.R. CHECK: J.W. 65-180
DATE: JAN 1962 LOADING: H2O, SIG. DRAWING No. D-4998-2

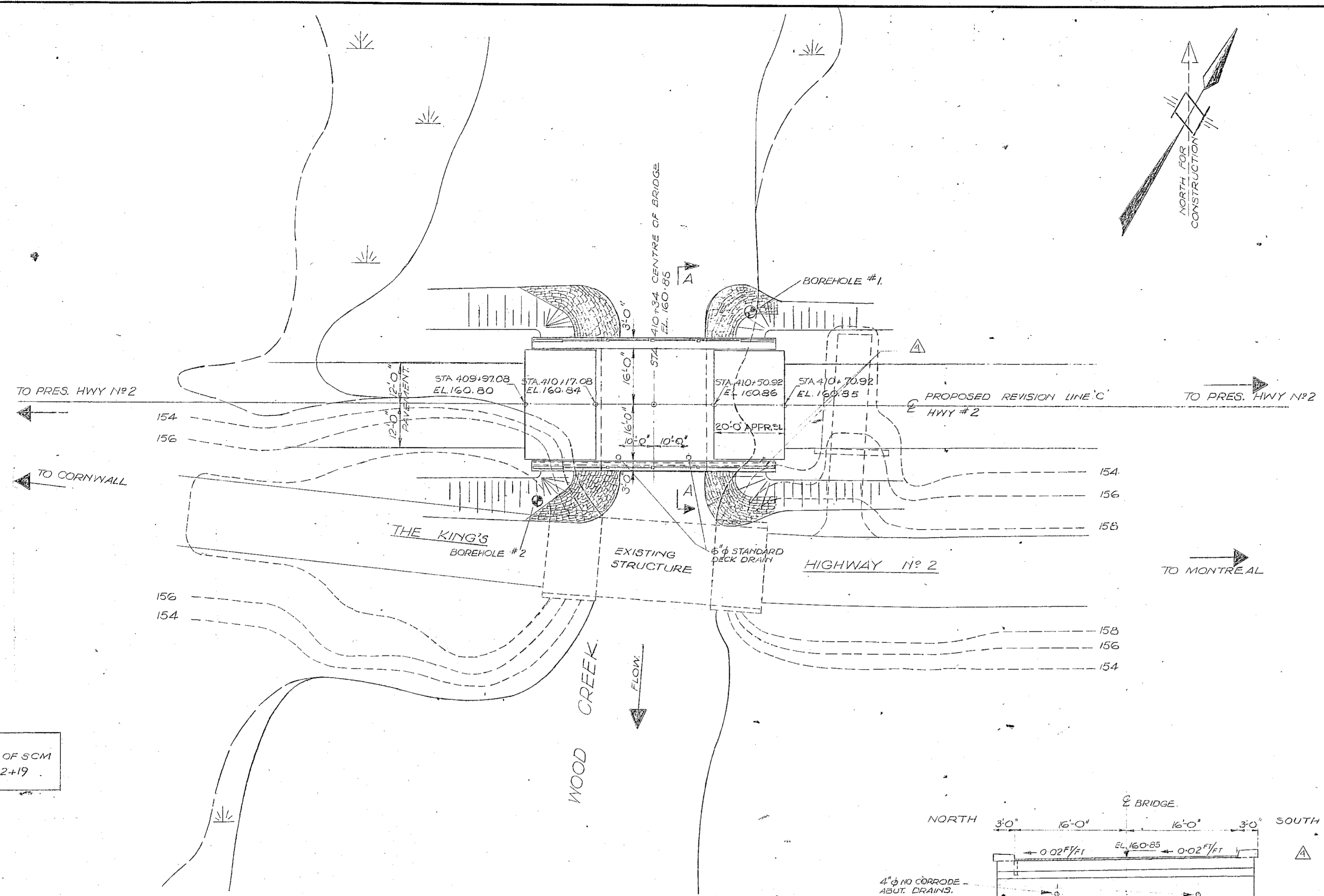
No.	FOR	DATE
2	PRINT	12-15-62



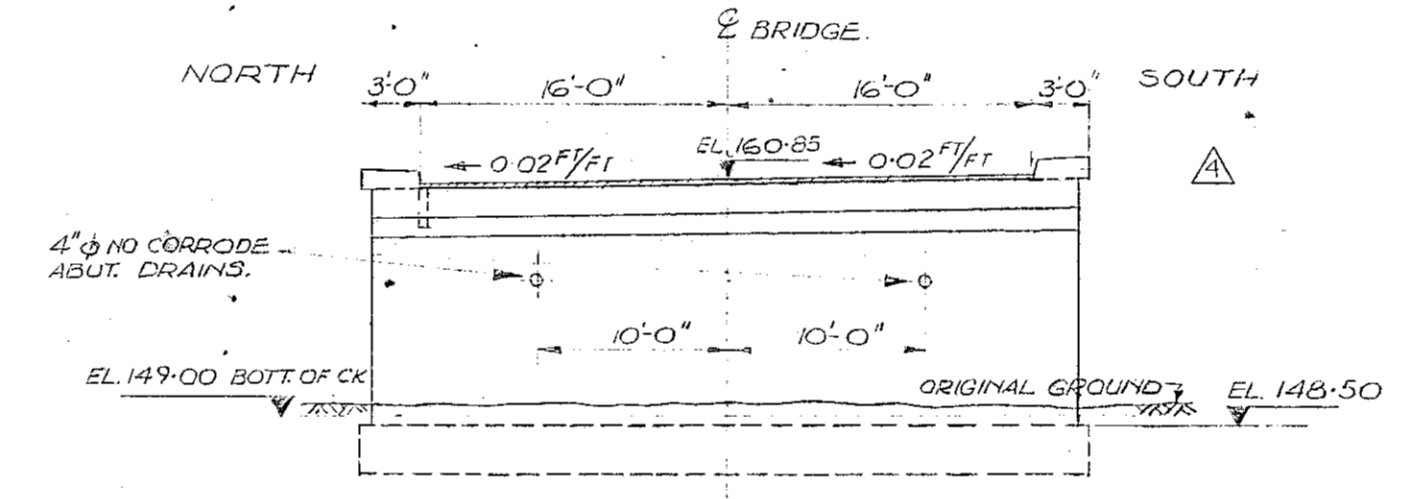
KEY PLAN
SCALE: 1 IN. = 0.8 M.



BOREHOLE DATA
SCALE 1 IN. = 20 FT.



PLAN
SCALE: 1 IN. = 20 FT.



SECTION A-A'
SCALE: 1 IN. = 10 FT.

- LIST OF DRAWINGS
- D-4996-1 - GENERAL PLAN
 - D-4996-2 - DETAILS OF FOOTINGS ABUT'S & DECK.
 - D-4996-3 - DETAILS OF APPR. SLAB WINGWALLS & ENDPOSTS.
 - D-4996-4 - REINFORCING STEEL SCHEDULE
 - D-4996-5 - " " " " " "
 - D-4996-6 - " " " " " "

NOTES

TO DISTRICT ENGINEER

CONCRETE WORK ON THIS STRUCTURE MUST NOT BE COMMENCED UNTIL MONUMENTS TO FIX CONTROL POINTS HAVE BEEN ERECTED AND CHECKED BY THE DISTRICT ENGINEER TO CONTRACTOR

STRUCTURE TO BE BUILT IN ACCORDANCE WITH FORMING AND THE SPECIAL PROVISIONS EXTRA COPIES OF WHICH MAY BE OBTAINED FROM THE DISTRICT ENGINEER

CONCRETE MIX

MAX. SIZE AGGREGATE 3/4"

STRENGTH 3000 PSI AT 28 DAYS

APPROVED ADMIXTURES SUPPLIED BY THE CONTRACTOR WILL BE ADDED TO ALL CONCRETE AS SPECIFIED BY THE ENGINEER

BOREHOLE DATA

THE COMPLETE SOIL INVESTIGATION REPORT FOR THIS STRUCTURE MAY BE EXAMINED AT THE BRIDGE OFFICE AND FOUNDATION OFFICE, DOWNSVIEW AT ANY REGIONAL OFFICE AND AT THE OTTAWA DISTRICT OFFICE. THE DEPARTMENT DOES NOT GUARANTEE THE ACCURACY OF THIS REPORT OR THE ABRIDGED VERSION SHOWN ON THESE PLANS.

FOOTINGS: 12" CLEAR COVER ON REINFORCING STEEL

ABUTMENTS: 12" DECK: 12" HANDRAILS: 12"

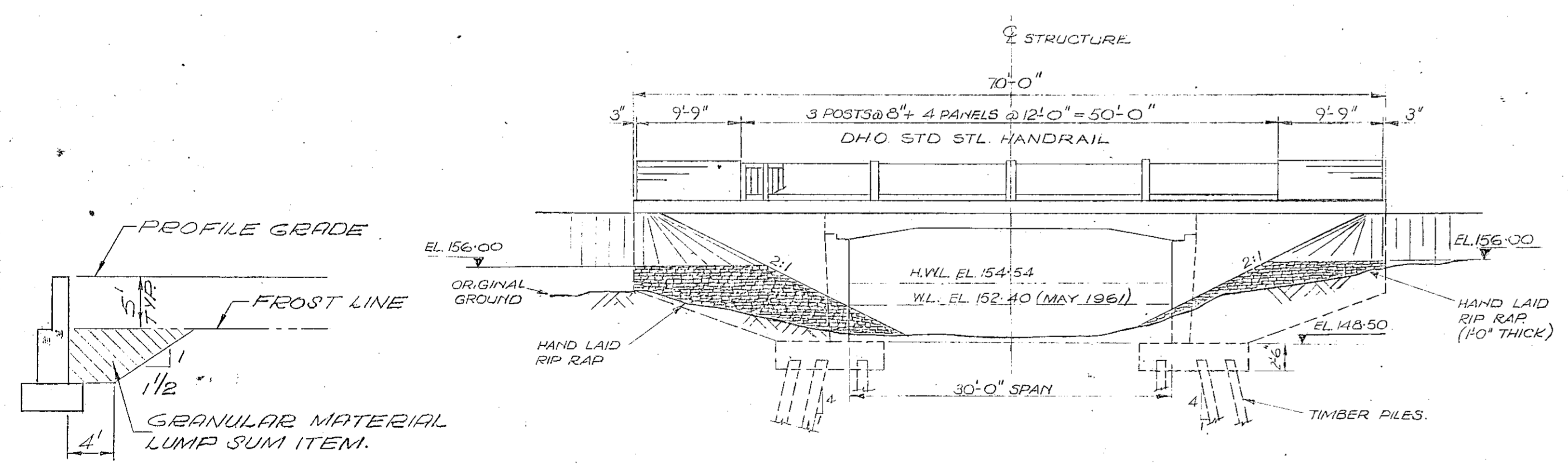
CONSTRUCTION NOTES

ALL EXPOSED EDGES TO BE CHAMFERED 1/4" EXCEPT AS NOTED.

ALL CONSTRUCTION JOINTS MUST BE APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE FINAL DECK ELEVATIONS CONFORM WITH THE ELEVATIONS SHOWN.

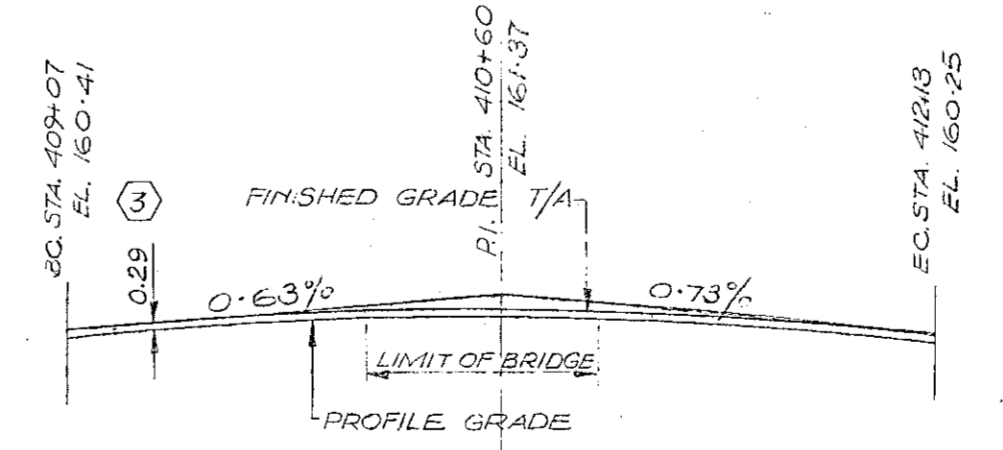
B.M. ELEVATION 154.79
GEODEIC DATUM TOP OF SCM
Nº 99 43' LT. OF STA. 412+19
PRES. HWY Nº 2.



ELEVATION
SCALE: 1 IN. = 10 FT.

DETAILS OF MINIMUM GRANULAR BACKFILL REQUIREMENTS

NOTE - SECTION 1 TO ABUTMENT LATERAL LIMITS - INSIDE FACE TO INSIDE FACE OF WINGWALLS.



PROFILE OF HWY #2 (NTS)

REF PLANS & REPORTS

- SITE PLAN - E 4002-1
- PLAN - 7833
- PROFILE - C778-2
- FOUNDATION REPORT - BA 1270
- HYDROLOGY REPORT - BW 373

REVISIONS	DATE	BY	DESCRIPTION
1	10-9-62	J.G.G.	GENERAL NOTE REVISED
2	5-15-65	R.T.	CHANGED 0.38 (4 1/2") TO 0.29 (3 1/2")
3	4-18-64	W.M.	GRANULAR BACKFILL DETAIL ADDED

DEPARTMENT OF HIGHWAYS ONTARIO
BRIDGE DIVISION

WOOD CREEK BRIDGE
(1.3 M. WEST OF QUEBEC BDRY.)

KING'S HIGHWAY No. 2 DIST. No. 9
CO. GLENGARRY
TWP. LANCASTER LOT 6 CON. I

GENERAL PLAN

APPROVED: [Signature] BRIDGE ENGINEER SITE No. W.P. No. 94-61

DESIGN: D.R.G. CHECK: J.R. CONTRACT No. 65-180

DRAWING: J.R. CHECK: J.W. DRAWING No. D4996-1

DATE: Jan 1962 LOADING: H20-S16

TWP # 33-239-1-C

Bid Items: The Bid Items are amended by addition of the following:

Bid Items

Item Number	Item Name	UOM	Quantity	Unit Price	Total	SP
PART D						
29	Site Work - Wood Creek Bridge	LS	1			SP14
30	Traffic Management - Wood Creek Bridge	LS	1			OPSS.MUNI 706, SP15
31	Dowels into Concrete - Wood Creek Bridge	ea.	98			OPSS.MUNI 905
32	Stainless Steel Bar - Wood Creek Bridge	t	0.2			OPSS.MUNI 905
33	Preformed Seals, Joint Fillers, Joint Seals, Joint Sealing Compounds, and Waterstops (Compression Seal) - Wood Creek	LS	1			OPSS.MUNI 920, SP16
34	Concrete Removal - Partial Depth - Type A - Wood Creek Bridge	m ³	0.8			OPSS.MUNI 928, SP18
35	Abrasive Blast Cleaning of Exposed Reinforcing Steel - Wood Creek Bridge	m ²	8			OPSS.MUNI 929, SP17
36	Two-component polyurethane Patch Material - Wood Creek	ea.	46			SP19